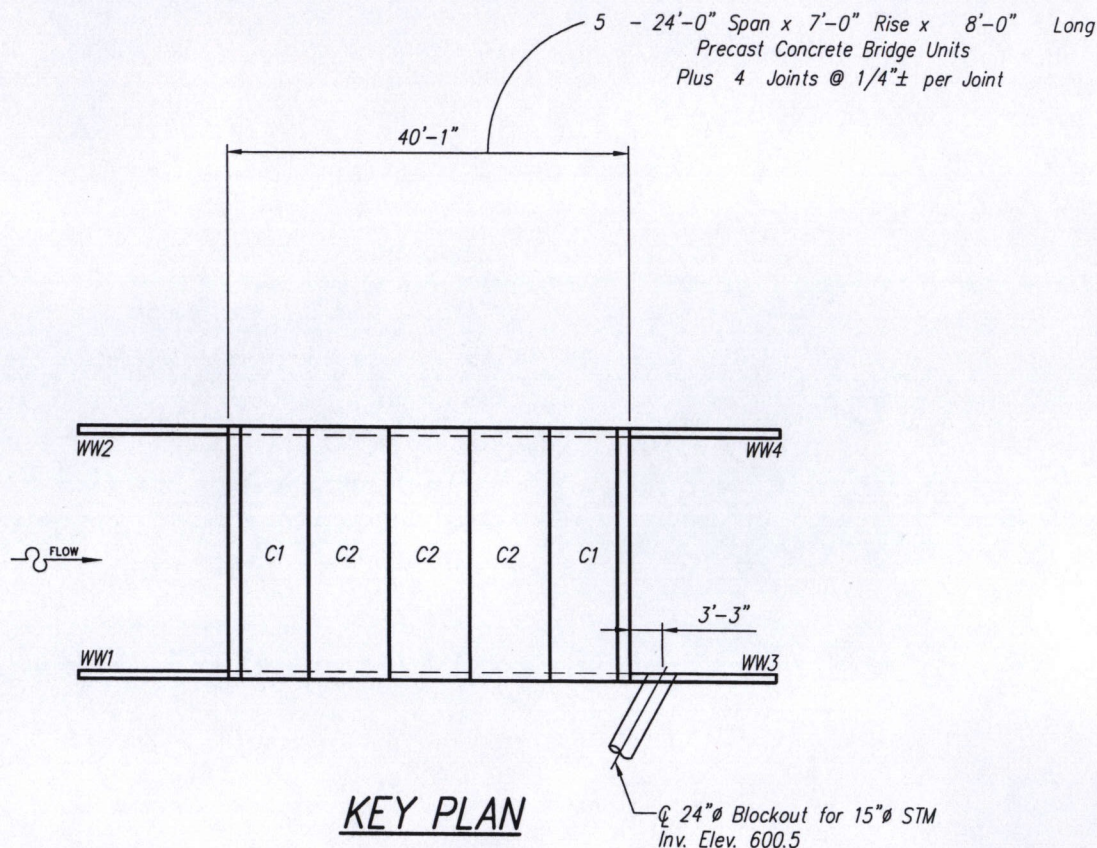


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Notes:

- All edges of Precast to have a 3/4" chamfer



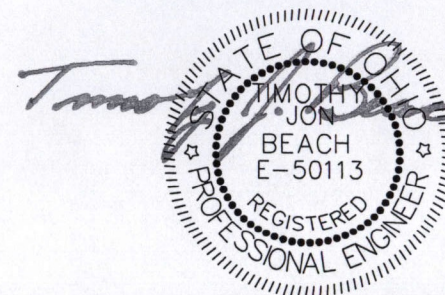
KEY PLAN

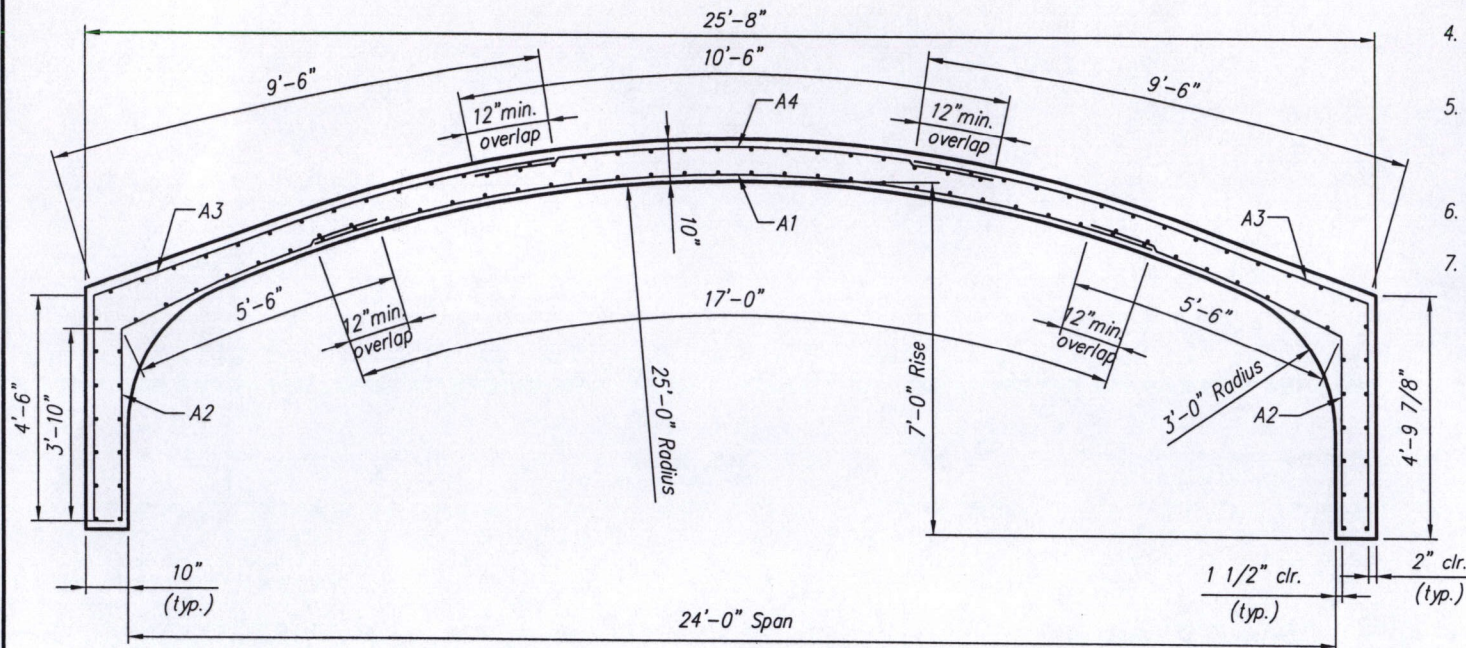
Revisions:		Date			
No.		1	2	3	4

Sheet Title:	KEY PLAN SHOP DRAWING
	Producer: BRIDGETEK OHIO 513-615-7474

OHIO	MORRIS FARM ACCESS BRIDGE OVER MIAMI & ERIE CANAL
------	---

Des. By: DLW	Job No. 11218
Drawn By: MGS	Sheet No.
Chk'd By: KTM	S1
Date: 03/15/04	





Weight of Required Reinforcement = 179.90 lbs/ft

Sheet no.	Circumferential Area Req'd (in ² /ft)	Longitudinal Area Req'd (in ² /ft)	Mesh Size	Length (ft)	Circumferential Area Supl'd (in ² /ft)	Longitudinal Area Supl'd (in ² /ft)
1	A1 = 0.84	0.17		17'-0"		
2	A2 = 0.24	0.13		9'-4"		
3	A3 = 0.72	0.13		14'-0"		
4	A4 = 0.24	0.24		10'-6"		
5						
6						
7						

Design Loading: HS20-44

Cover = 1'-8" min. \ 2'-0" max.

NOTES:

1. Minimum 28-Day Concrete Compressive Strength shall be 4000 psi.
2. Overlap Length shall be measured from last crosswire.
3. Dimensions shown are for form system "A".
4. Minimum yield strength for welded wire fabric shall be 65,000 psi.
5. Reinforcing shall be limited to a maximum of three layers of reinforcing (WWF or bars) per area (A1, A2, A3 or A4).
6. All edges of Precast to have a 3/4" chamfer.
7. Spacing of longitudinal reinforcement must be a maximum of 8" o.c. For multiple layers of mesh, only the outer most layer (A1a or A3a) must be a maximum of 8" o.c.

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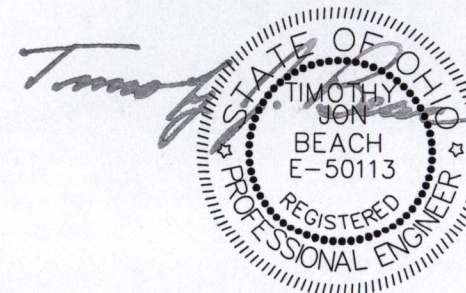
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Revisions:	Date	No.	1	2	3	4

Sheet Title:	PRECAST UNIT REINFORCEMENT SHOP DRAWING
Producer:	BRIDGETEK OHIO 513-615-7474

OHIO
WEST CHESTER
MORRIS FARM
ACCESS BRIDGE
OVER MIAMI & ERIE CANAL

Des. By:	DLW	Job No.	11218
Drawn By:	MGS	Sheet No.	S2
Chk'd By:	KTM		
Date:	03/15/04		



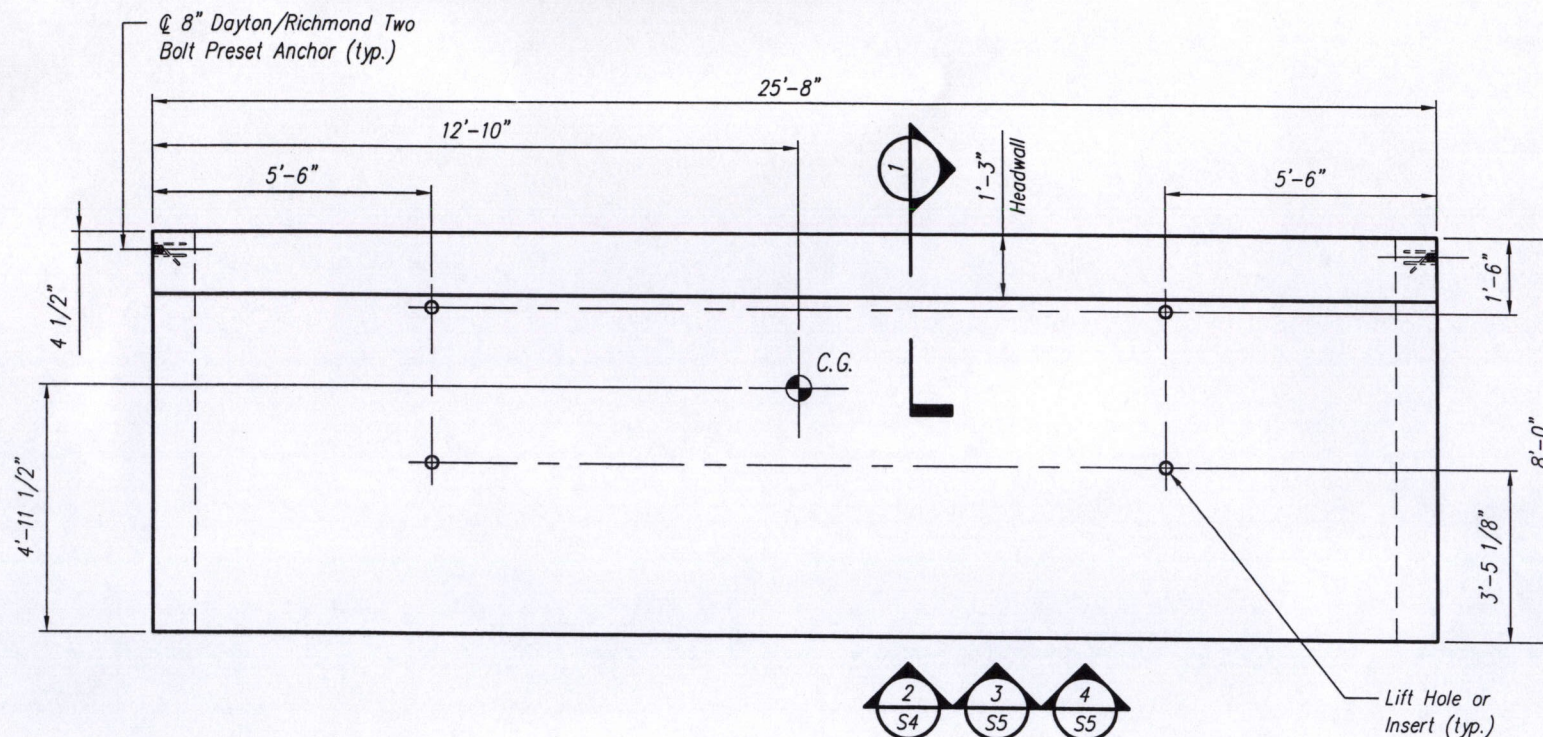
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Revisions:	Date			
	No.	1	2	3

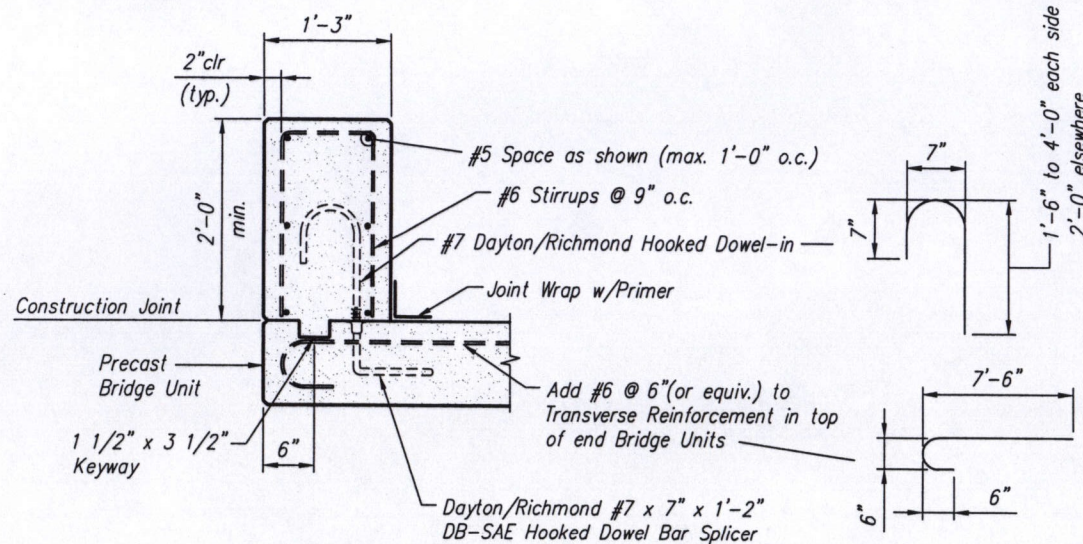
Sheet Title:	HEADWALL END UNIT - C1			
	SHOP DRAWING			
Producer:	BRIDGETEK			
	OHIO			
	513-615-7474			

WEST CHESTER	OHIO			
	MORRIS FARM			
	ACCESS BRIDGE			
	OVER MIAMI & ERIE CANAL			

Des. By:	DLW	Job No.	11218
Drawn By:	MGS	Sheet No.	S3
Chk'd By:	KTM		
Date:	03/15/04		



PLAN



SECTION



Notes:
- All edges of Precast to have a 3/4" chamfer

Unit Weight = 18.5 Tons
Headwall Weight = 7.4 Tons
Total Weight = 25.9 Tons

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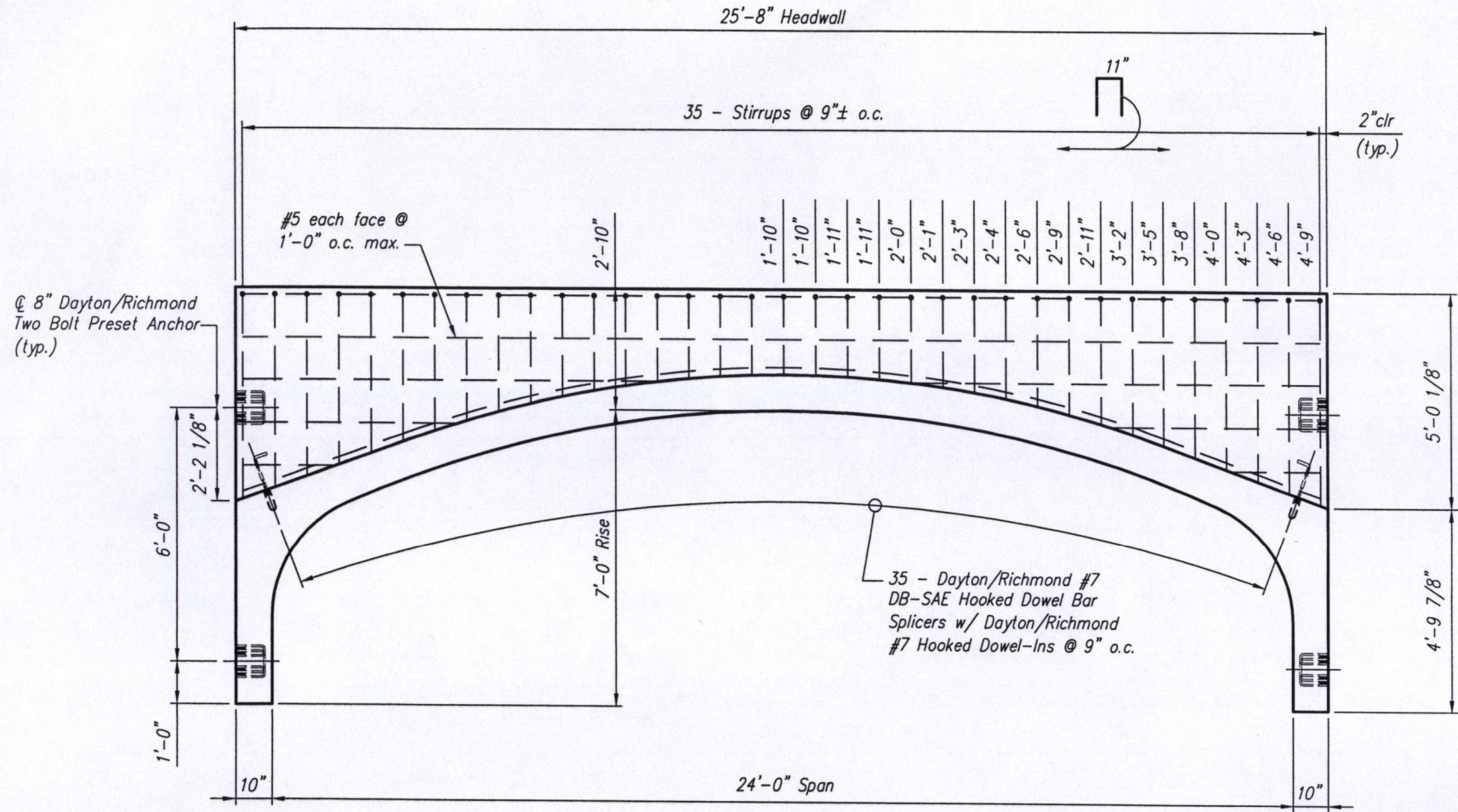
Revisions:	Date			
	No.	1	2	3

Sheet Title:	HEADWALL REINFORCING - C1
	SHOP DRAWING
Producer:	BRIDGETEK
	OHIO
	513-615-7474

WEST CHESTER	OHIO
	MORRIS FARM
	ACCESS BRIDGE
	OVER MIAMI & ERIE CANAL

Des. By:	DLW	Job No.	11218
Drawn By:	MGS	Sheet No.	
Chk'd By:	KTM		
Date:	03/15/04		

S4



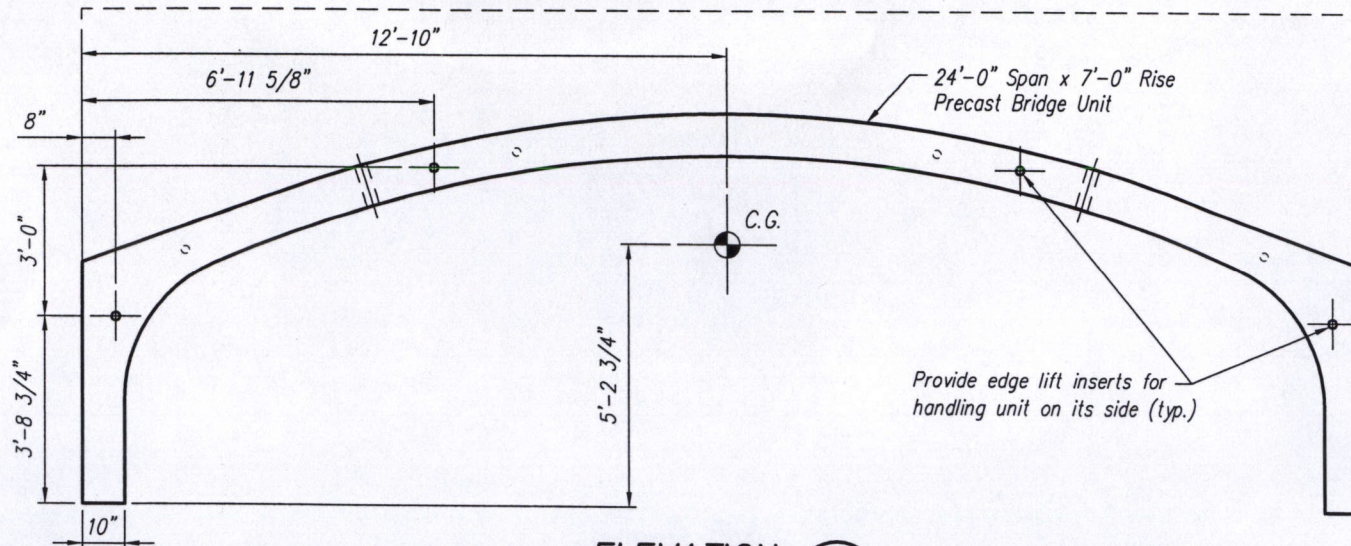
ELEVATION 2 S3

Headwall		Arch Unit		
Concrete	Reinf. Steel	Concrete	Reinf. Steel	WWF
28-day, 4000 psi	60,000 psi uncoated	28-day, 4000 psi	60,000 psi uncoated	65,000 psi uncoated

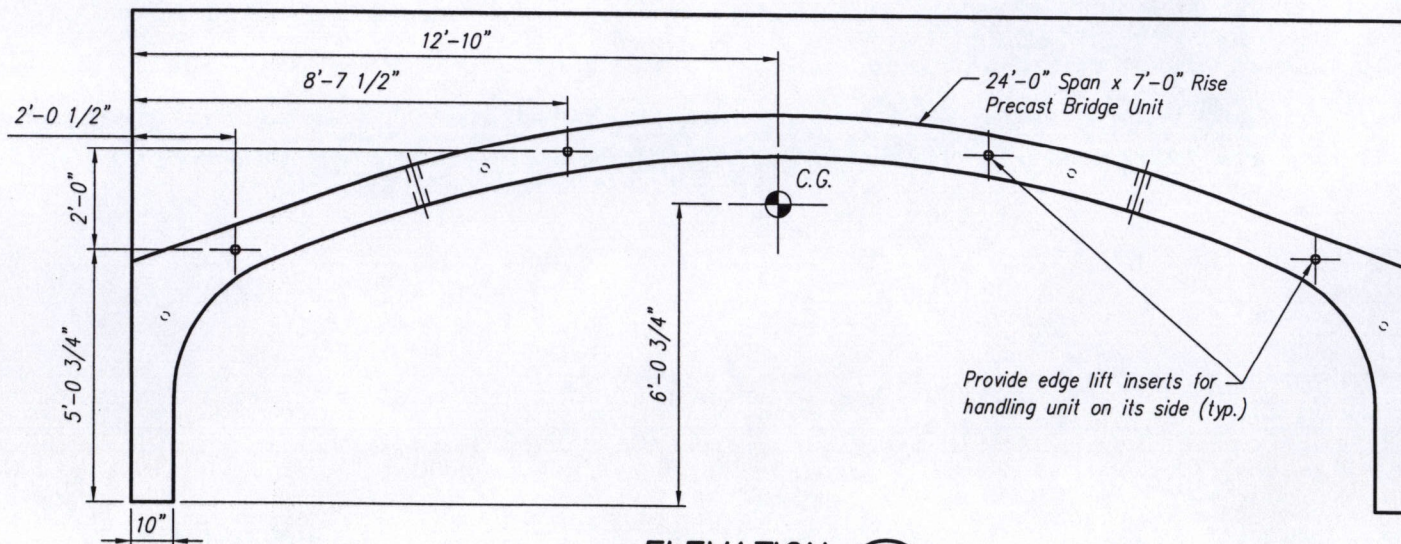
Wherever the reinforcing is cut for the placement of lift holes or other blockouts, reinforcing bars or wires of equivalent cross-sectional area shall be placed symmetrically around the hole. At least one bar must be on each side of the hole, and the development length of the bar must be achieved on either side of the cut.

Notes:

- All edges of Precast to have a 3/4" chamfer
- Elevation is looking at back face of Headwall
- See Sheet S5 for C.G. & Edge Lift Points
- Units must be grouted or braced when setting precast headwalls



ELEVATION 3
WITHOUT HEADWALL S3



ELEVATION 4
WITH HEADWALL S3

Notes:

- All edges of Precast to have a 3/4" chamfer

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Sheet Title:	C.G. & EDGE LIFT POINTS - C1 SHOP DRAWING			
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WEST CHESTER	OHIO			
	MORRIS FARM ACCESS BRIDGE OVER MIAMI & ERIE CANAL			

Des. By:	DLW	Job No.	11218
Drawn By:	MGS	Sheet No.	
Chk'd By:	KTM		
Date:	03/15/04		

S5

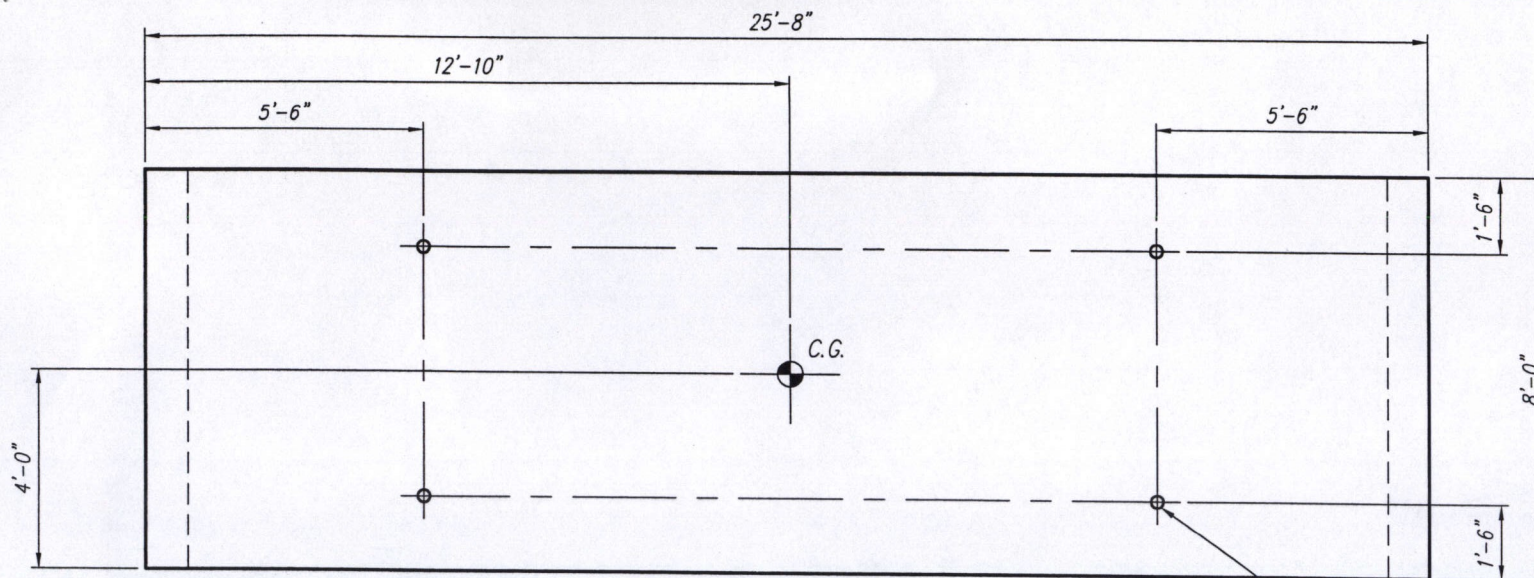
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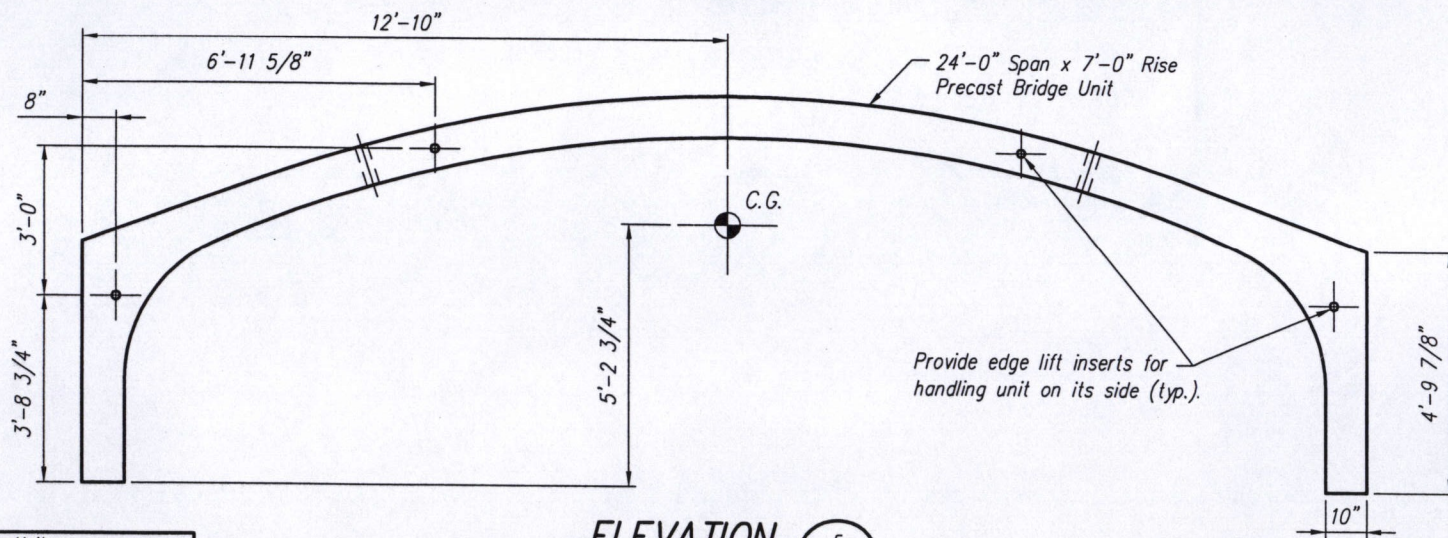
Sheet Title:	INTERIOR UNIT - C2 SHOP DRAWING
	Producer: BRIDGETEK OHIO 513-615-7474

WEST CHESTER	MORRIS FARM ACCESS BRIDGE OVER MIAMI & ERIE CANAL
	OHIO

Des. By: DLW	Job No. 11218
Drawn By: MGS	Sheet No.
Chk'd By: KTM	S6
Date: 03/15/04	



PLAN



ELEVATION

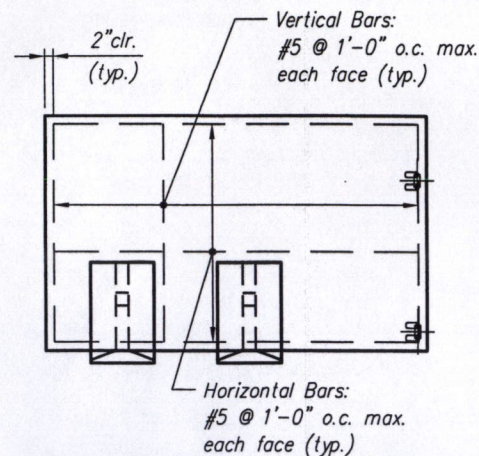
Arch Unit		
Concrete	Reinf. Steel	WWF
28-day: 4000 psi	60,000 psi uncoated	65,000 psi uncoated

Wherever the reinforcing is cut for the placement of lift holes or other blockouts, reinforcing bars or wires of equivalent cross-sectional area shall be placed symmetrically around the hole. At least one bar must be on each side of the hole, and the development length of the bar must be achieved on either side of the cut.

Notes:
- All edges of Precast to have a 3/4" chamfer

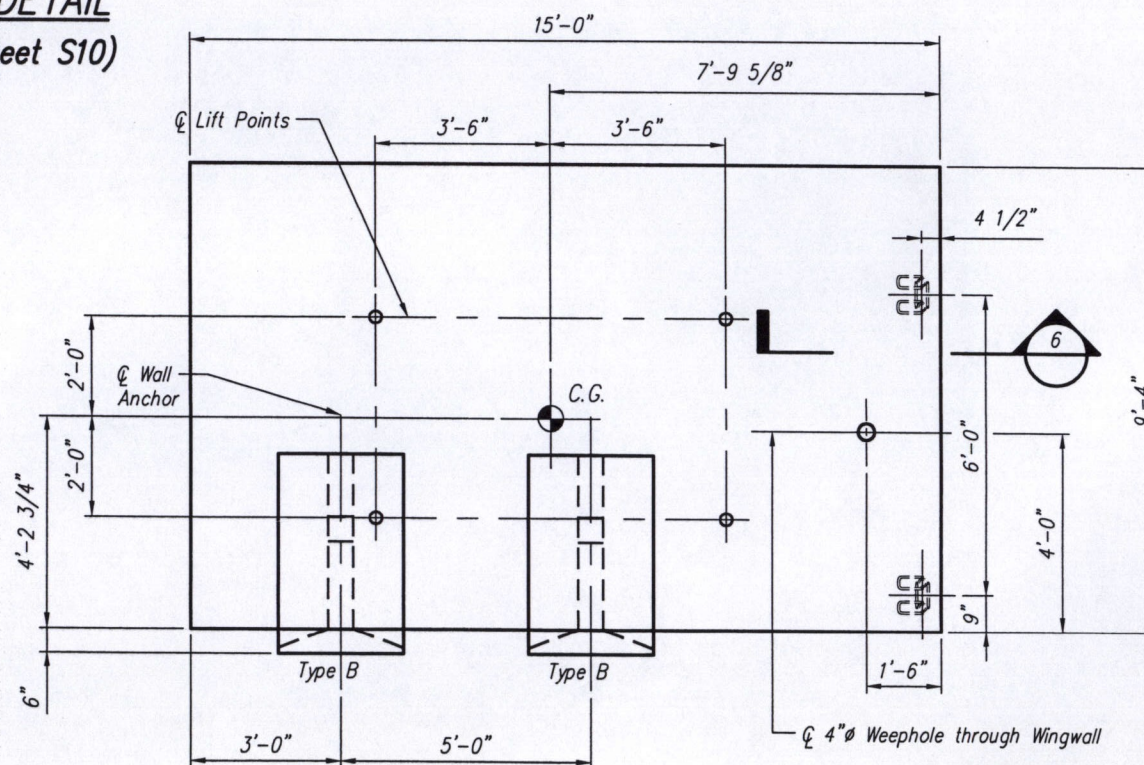
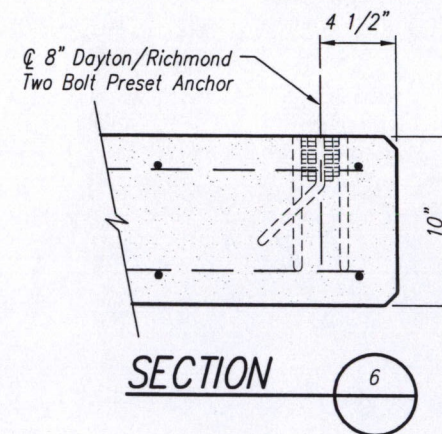
Total Weight = 18.5 Tons

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REINFORCING DETAIL

(See Section - Sheet S10)



Wingwall	
Concrete	Reinf. Steel
28-day 4000 psi	60,000 psi uncoated

Wherever the reinforcing is cut for the placement of lift holes or other blockouts, reinforcing bars or wires of equivalent cross-sectional area shall be placed symmetrically around the hole. At least one bar must be on each side of the hole, and the development length of the bar must be achieved on either side of the cut.

ELEVATION

Notes:

- All edges of Precast to have a 3/4" chamfer

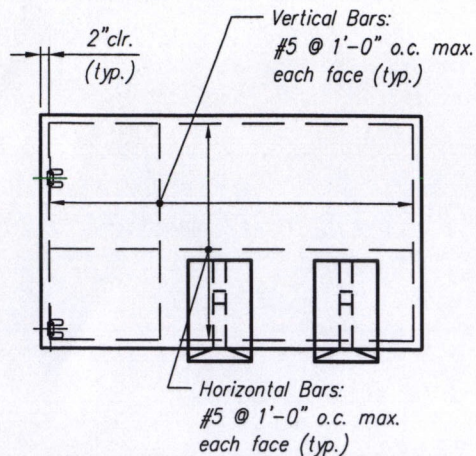
Total Weight = 10.3 Tons

Revisions:	Date		No.	1	2	3	4

Sheet Title:	WINGWALL WW1 & WW4 SHOP DRAWING
Producer:	BRIDGETEK OHIO 513-615-7474

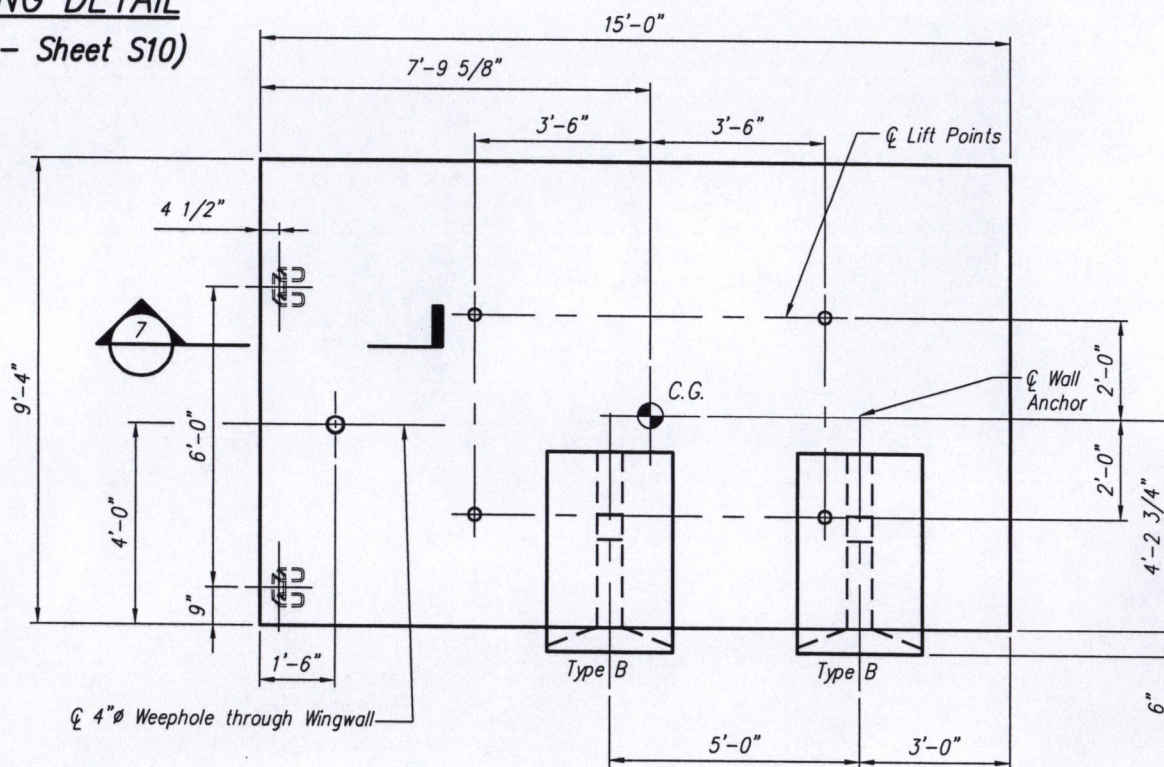
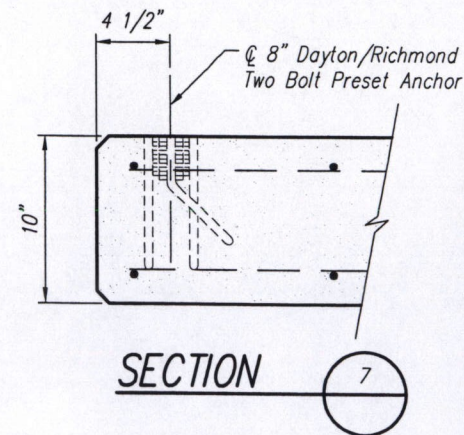
WEST CHESTER	MORRIS FARM ACCESS BRIDGE OVER MIAMI & ERIE CANAL
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Des. By:	DLW	Job No.	11218
Drawn By:	MGS	Sheet No.	S7
Chk'd By:	KTM		
Date:	03/15/04		



REINFORCING DETAIL

(See Section - Sheet S10)



ELEVATION

Notes:

- All edges of Precast to have a 3/4" chamfer

Total Weight = 10.3 Tons

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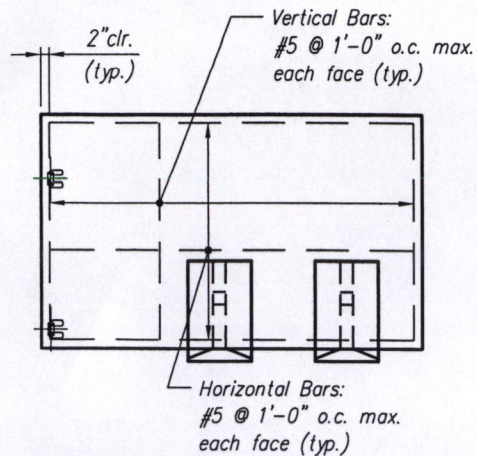
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	BRIDGETEK OHIO 513-615-7474			

WEST CHESTER	OHIO			
	MORRIS FARM ACCESS BRIDGE OVER MIAMI & ERIE CANAL			

Des. By: DLW	Job No. 11218
Drawn By: MGS	Sheet No.
Chk'd By: KTM	S8
Date: 03/15/04	

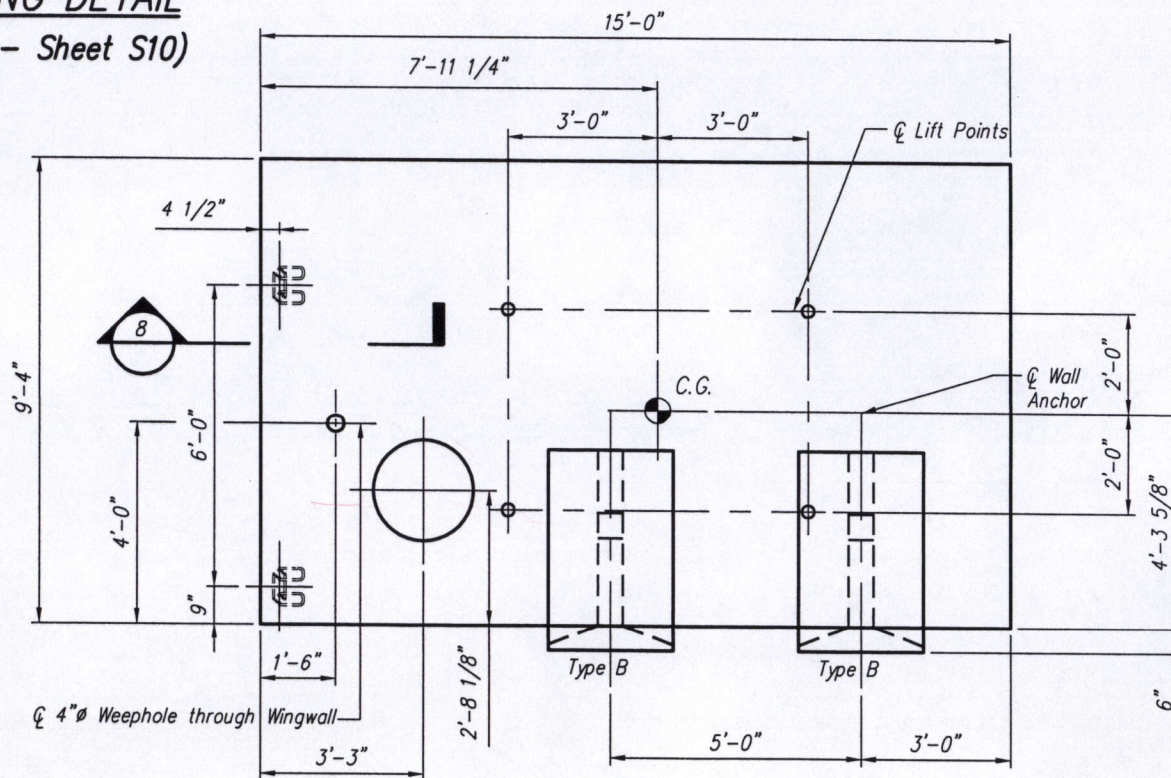
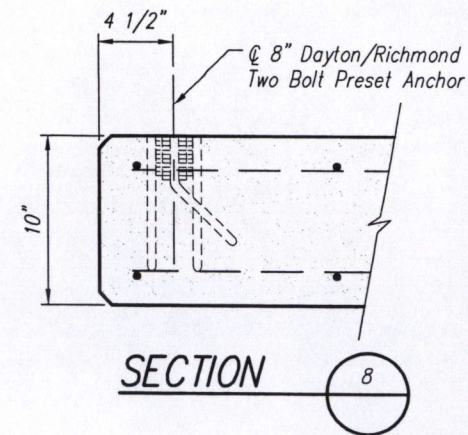
Wingwall	
Concrete	Reinf. Steel
28-day: 4000 psi	60,000 psi uncoated

Wherever the reinforcing is cut for the placement of lift holes or other blockouts, reinforcing bars or wires of equivalent cross-sectional area shall be placed symmetrically around the hole. At least one bar must be on each side of the hole, and the development length of the bar must be achieved on either side of the cut.



REINFORCING DETAIL

(See Section - Sheet S10)



Wingwall	
Concrete	Reinf. Steel
28-day: 4000 psi	60,000 psi uncoated

Wherever the reinforcing is cut for the placement of lift holes or other blockouts, reinforcing bars or wires of equivalent cross-sectional area shall be placed symmetrically around the hole. At least one bar must be on each side of the hole, and the development length of the bar must be achieved on either side of the cut.

ELEVATION

Notes:

- All edges of Precast to have a 3/4" chamfer

Total Weight = 10.1 Tons

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Sheet Title:	WINGWALL WW3 SHOP DRAWING	
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WEST CHESTER	OHIO	
	MORRIS FARM ACCESS BRIDGE OVER MIAMI & ERIE CANAL	

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Drawn By: MGS	Sheet No.
Chk'd By: KTM	S9
Date: 03/15/04	

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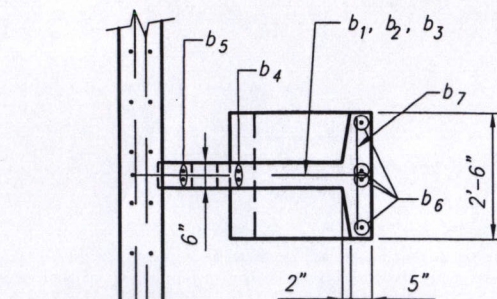
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	Producer: BRIDGETEK OHIO 513-615-7474	

WEST CHESTER	OHIO	
	MORRIS FARM ACCESS BRIDGE OVER MIAMI & ERIE CANAL	

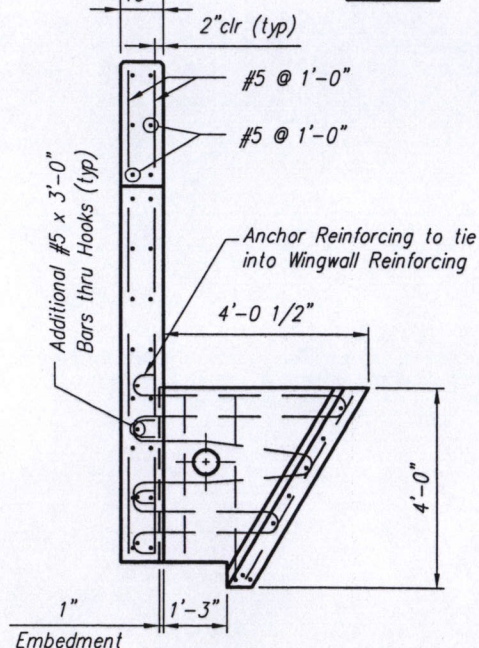
Des. By: DLW	Job No. 11218
Drawn By: MGS	Sheet No.
Chk'd By: KTM	S10
Date: 03/15/04	

BAR LIST					
MARK	QTY.	SIZE	L	Type	LENGTH
b ₁	1	#5	2'-10"	1	—
b ₂	1	#5	3'-6"	3	—
b ₃	1	#5	4'-2"	1	—
b ₄	2	#5	3'-8"	2	—
b ₅	2	#5	—	Str.	3'-2"
b ₆	4	#5	—	Str.	4'-2"
b ₇	7	#5	—	Str.	2'-2"

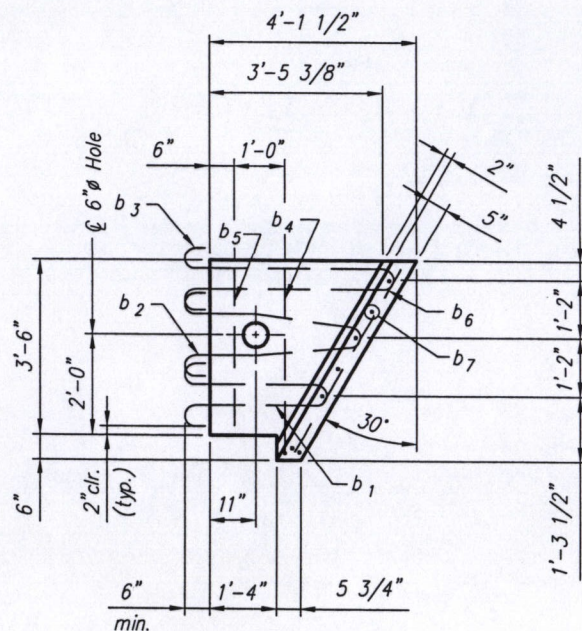
Note: "Str." denotes straight bar.
Standard Clearance = 2"



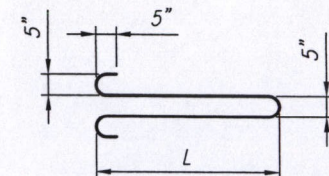
PLAN



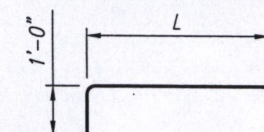
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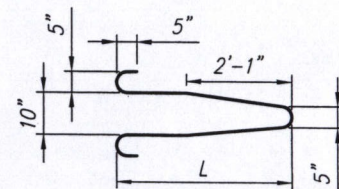
**PRECAST ANCHOR
TYPE B**



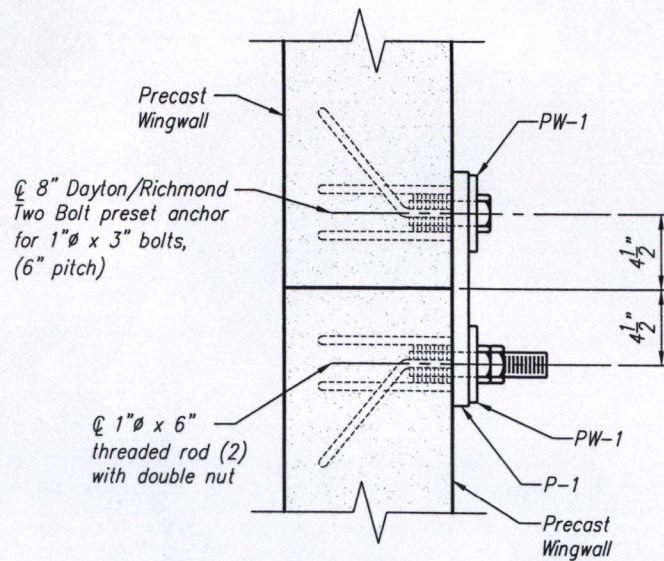
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Type 2

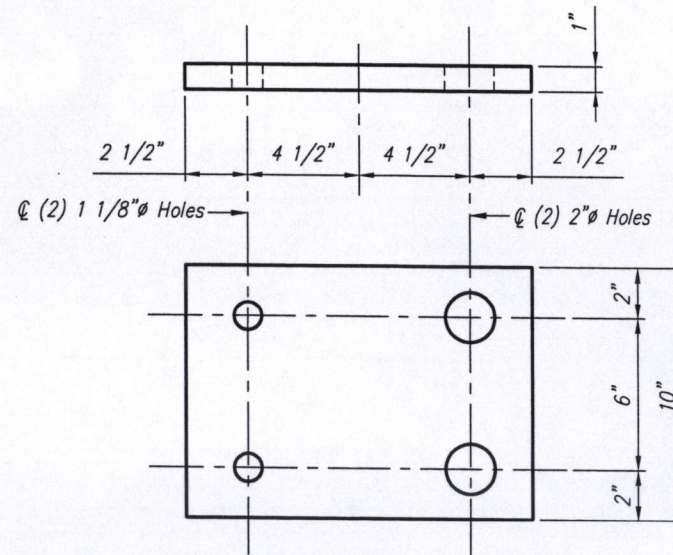


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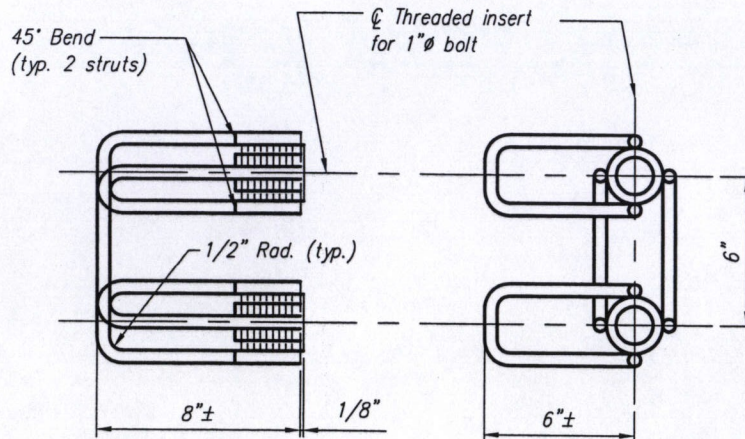
Plan View

Typical Connection Detail



P-1

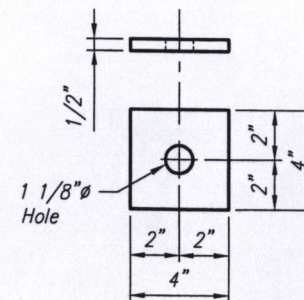
(⌀ 1" x 10" x 14")
(Galvanized as per ASTM A153)



Side View

Front View

Dayton/Richmond Two Bolt Preset Anchor



PW-1

(⌀ washer, 1/2" x 4" x 4")
(Galvanized as per ASTM A153)

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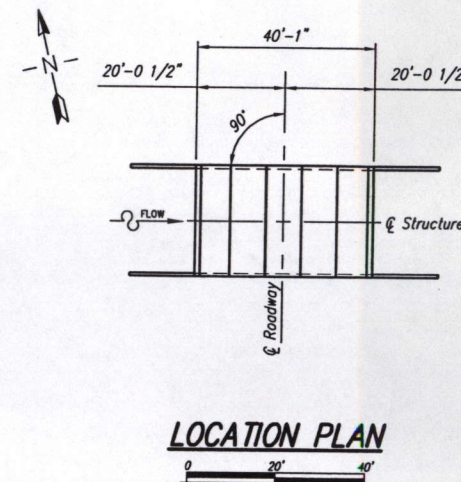
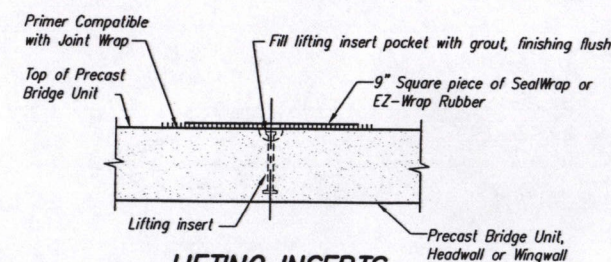
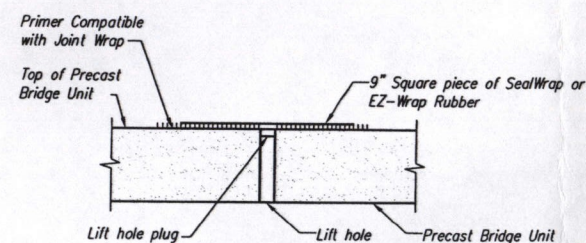
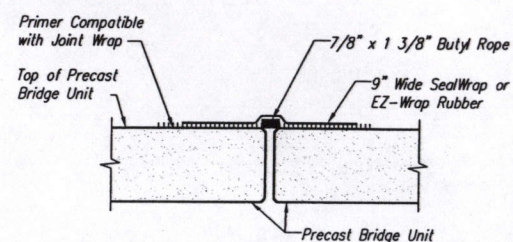
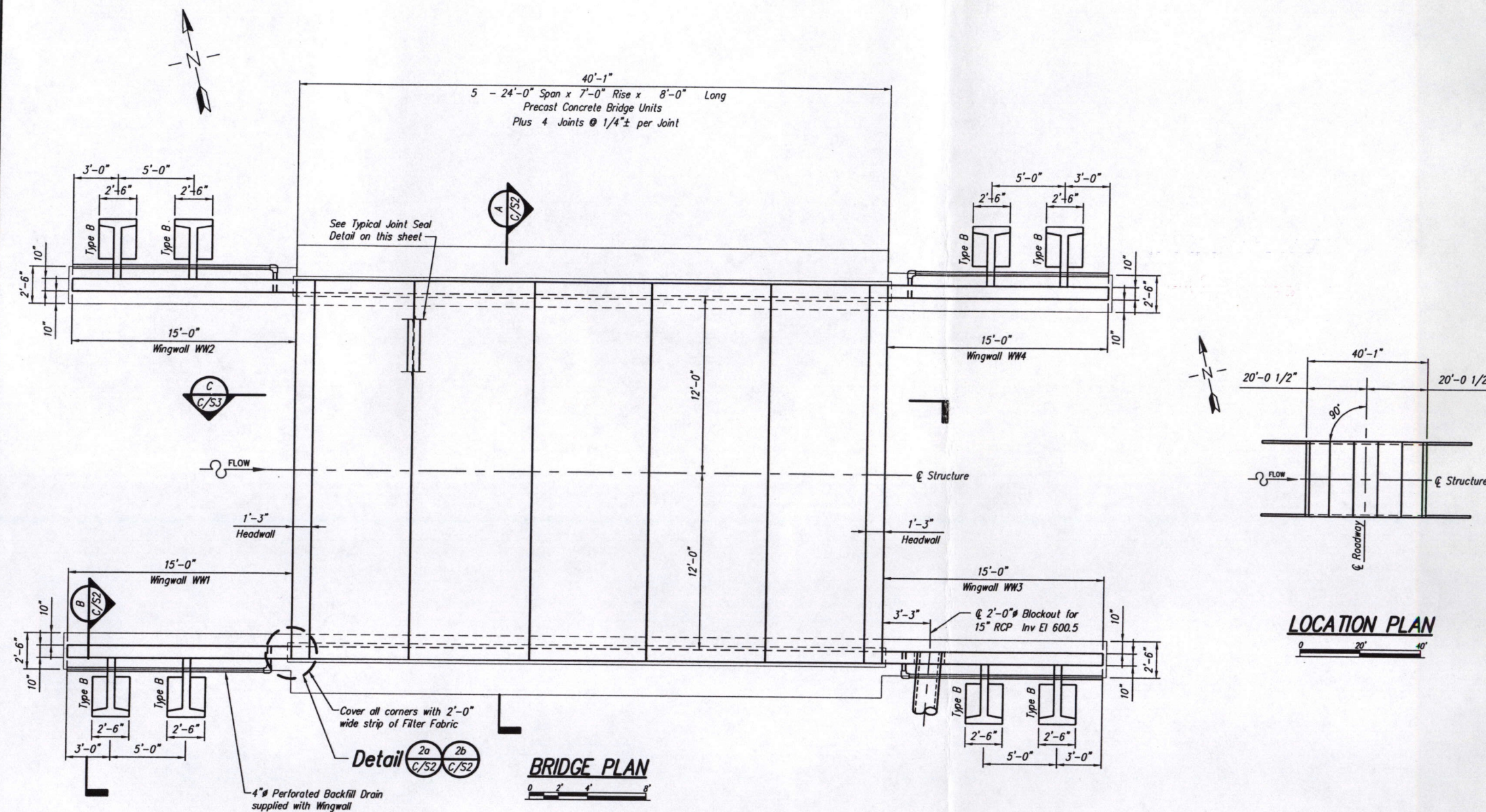
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	Producer: BRIDGETEK OHIO 513-615-7474	

WEST CHESTER	OHIO	
	MORRIS FARM ACCESS BRIDGE OVER MIAMI & ERIE CANAL	

Des. By: DLW	Job No. 11218
Drawn By: MGS	Sheet No.
Chk'd By: KTM	S11
Date: 03/15/04	

PLATE P-1

Total Required = 8
(4) PW-1 Req'd. per Plate



NOTES

GENERAL NOTES:

1. This bridge has been designed for general site conditions. The project engineer shall be responsible for the structure's suitability to the existing site conditions and for the hydraulic evaluation -- including scour and confirmation of soil conditions.
2. Prior to construction, contractor must verify all elevations shown through the engineer.
3. Only a qualified and CON/SPAN® approved precaster may provide the structure designed in accordance with these plans.
4. The use of another precast structure with the design assumptions used for the CON/SPAN® structure may lead to serious design errors. Use of any other precast structure with this design and drawings voids any certification of this design and warranty. CON/SPAN® Bridge Systems Ltd. assumes no liability for design of any alternate or similar type structures.

DESIGN DATA

Design Loading:

Bridge Units: HS20-44

Headwalls: Earth Pressure + Vehicular Impact

Wingwalls: Earth Pressure Only

Design Fill Height: Varies from 1'-8" min. to 2'-0" max.
from top of crown to top of pavement.

Design Method: Load factor per AASHTO Specification

Assumed Allowable soil Bearing: 3000 PSF (Verify)

MATERIALS

Precast units shall be constructed and installed in accordance with CON/SPAN® Specifications. Concrete for Footings shall have a minimum compressive strength of 4000 psi. Reinforcing steel for footings shall conform to ASTM 615, A616 or A617—Grade 60.

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OHIO

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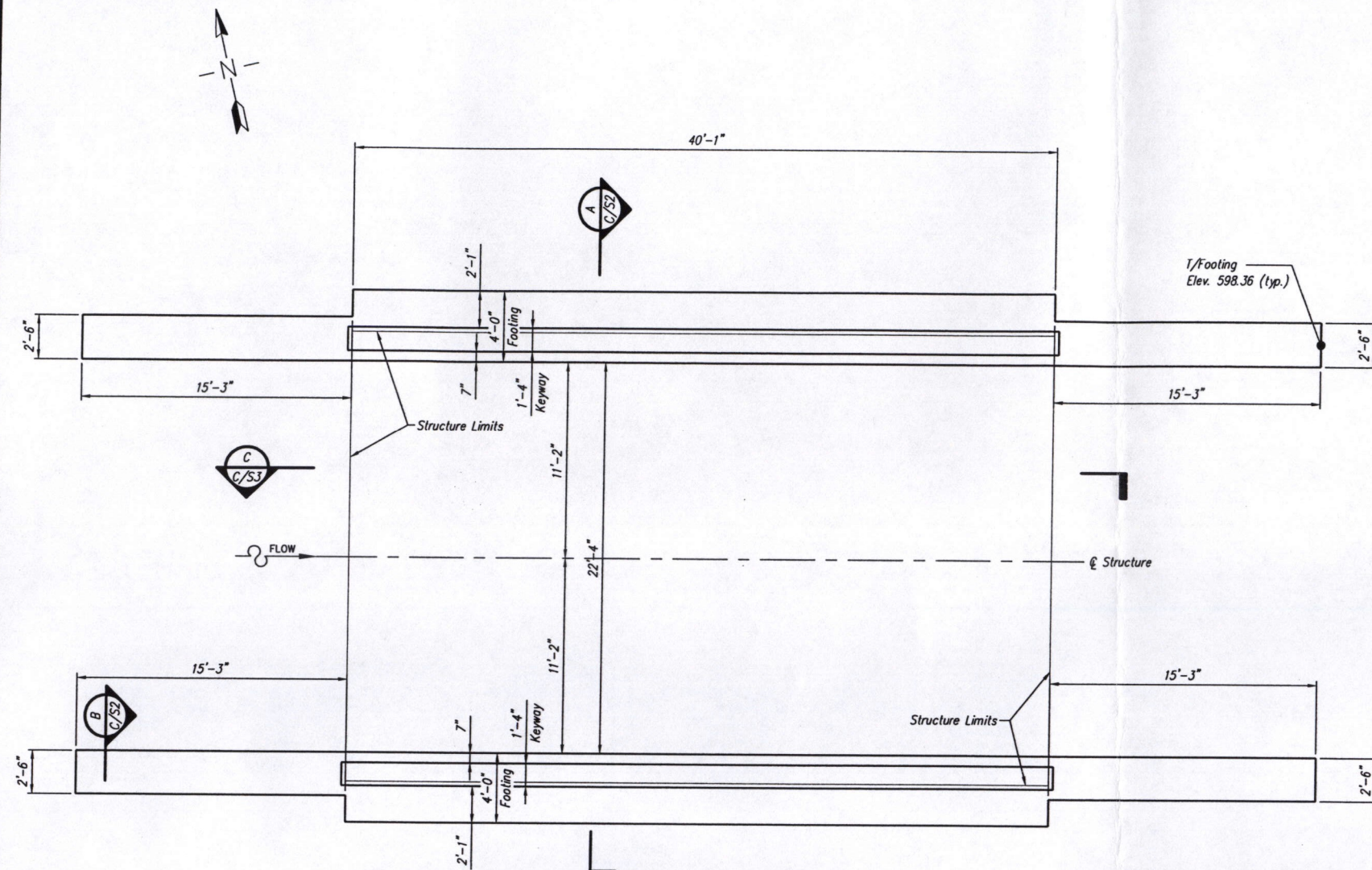
838
Canton, OH
Tollis, NC
Paramounts, CA
New York, NY

OHIO

**MORRIS FARM
ACCESS BRIDGE
OVER MIAMI & ERIE CANAL**

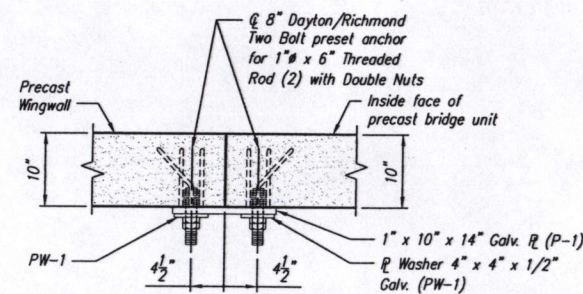
CHESTER

Designed DLW	C/S Project No. 11218
Drawn MGS	
Checked KTM	Sheet No.
Date 03/12/04	C/S1



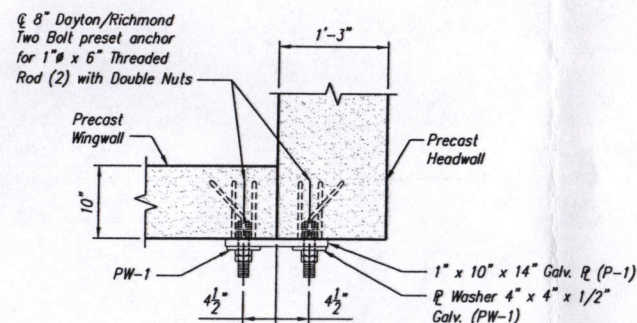
FOUNDATION PLAN

Note:
Lap (3'-0") #6 Longitudinal Bars in Wingwall
and Bridge Footings to make continuous



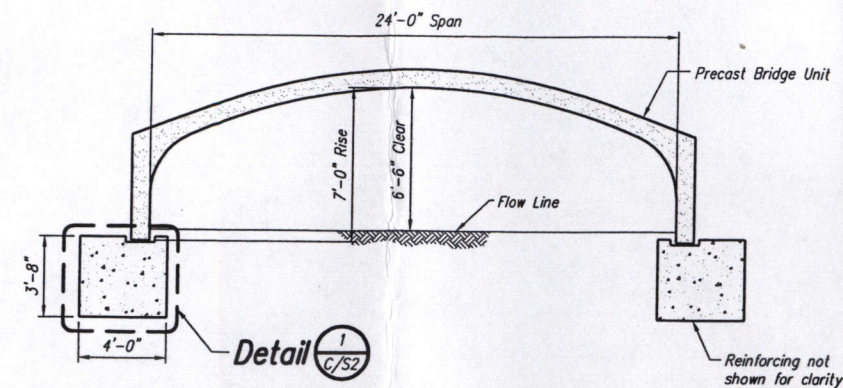
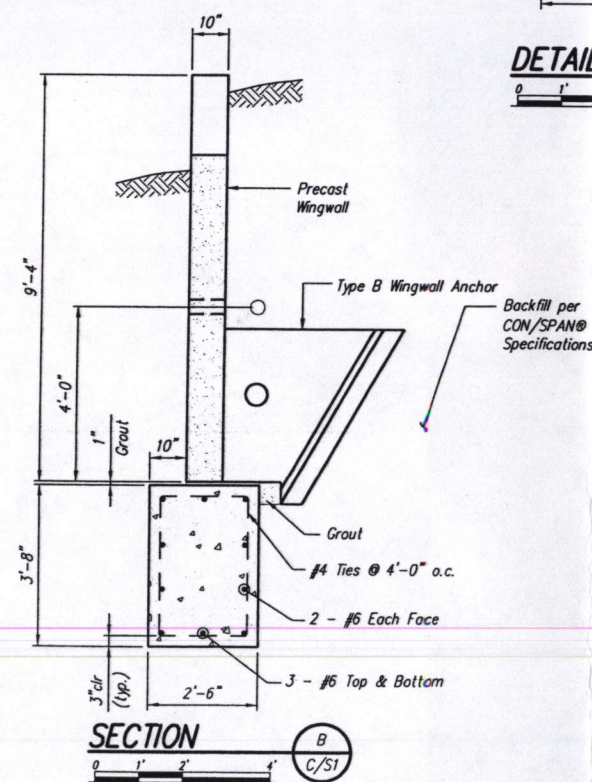
Note:
Connection R 's ($P-1$)
must be positioned with
small ϕ holes toward
precast bridge unit

DETAIL © Unit Lea

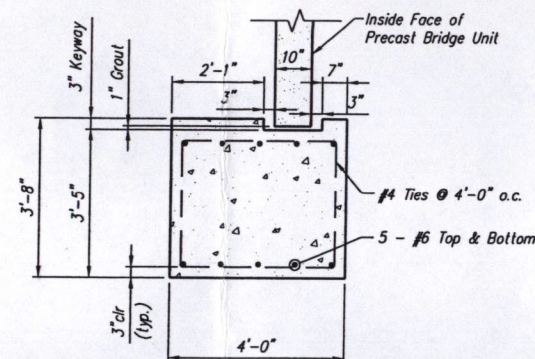


Note:
Connection R's (P-1)
must be positioned with
small ϕ holes toward
precast Headwall

DETAIL • Headwall



SECTION



DETAIL

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No.	Date
REVISIONS	

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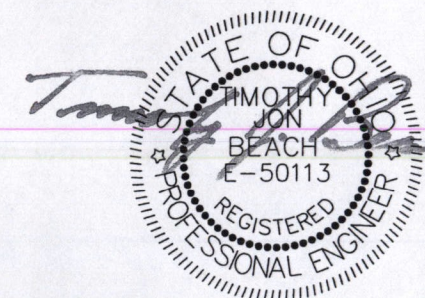
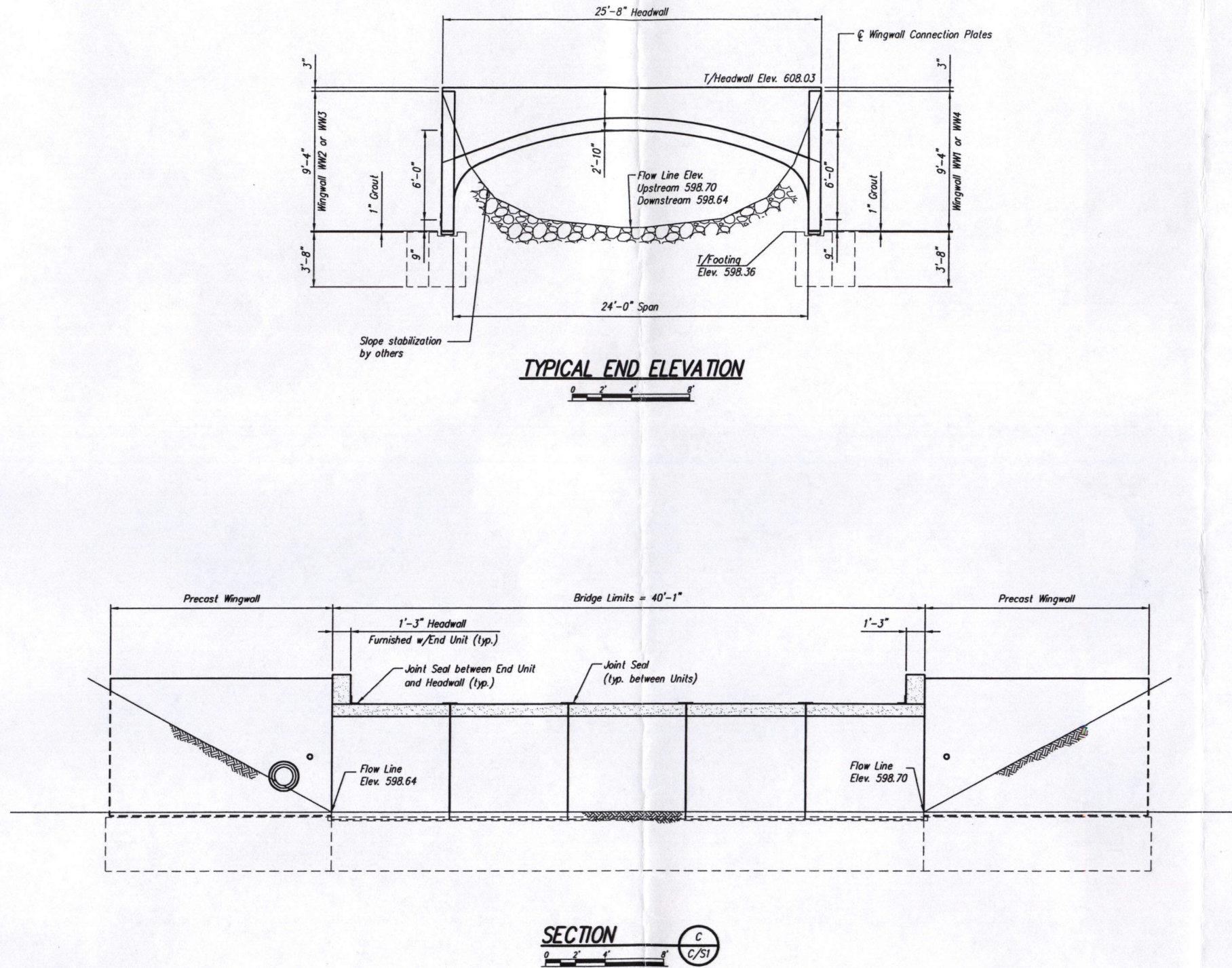
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OHIO

**MORRIS FARM
ACCESS BRIDGE
OVER MIAMI & ERIE CANAL**

Designed DLW	C/S Project No. 11218
Drawn MGS	
Checked KTM	Sheet No. C/S2
Date 03/12/04	



NOTES

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MORRIS FARM

ACCESS BRIDGE

OVER MIAMI & ERIE CANAL

WESTCHESTER

DESIGNED: DLW

DRAWN: MGS

CHECKED: KTM

DATE: 03/12/04

C/S Project No. 11218

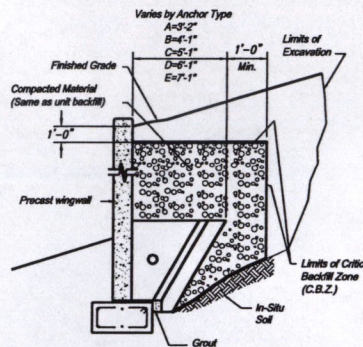
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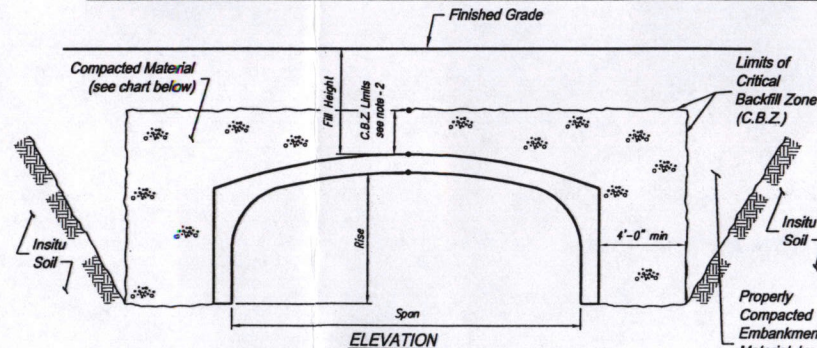
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BACKFILL REQUIREMENTS



WINGWALL BACKFILL REQUIREMENTS

BACKFILL DESCRIPTION (AASHTO M 145-91)								
Group Classification	A-1		A-3	A-2				A-4
	A-1-a	A-1-b		A-2-4	A-2-5	A-2-6	A-2-7	
Sieve Analysis, Percent Passing (100% Passing 3" Sieve)								
No. 10	50 max.							
No. 40	30 max.	50 max.	51 min.					
No. 200	15 max.	25 max.	10 max.	35 max.	35 max.	35 max.	35 max.	36 min.
Characteristics of Fraction Passing								
No. 40								
Liquid Limit				40 max.	41 min.	40 max.	41 min.	40 max.
Plasticity Index				10 max.	10 max.	11 min.	11 min.	10 max.
Usual Types of Significant Constituent Materials	6 max.	Stone Fragments, Gravel & Sand	N.P. Fine Sand	10 max.	10 max.	11 min.	11 min.	10 max.
General Rating as Subgrade			Excellent to Good					Fair to Poor



NOTES

1. SEE CONSPAN® SPECIFICATIONS SECTION 15.4 FOR BACKFILL SPECIFICATIONS.
2. FOR FILL HEIGHTS GREATER THAN 2'-0", C.B.Z. LIMIT SHALL BE 2'-0" ABOVE ARCH CROWN. FOR FILL HEIGHTS LESS THAN 2'-0", THE FINISHED GRADE SHALL BE THE BOUNDARY LINE FOR THE C.B.Z.
3. BACKFILLING OPERATIONS WITHIN THE C.B.Z. SHALL BE PERFORMED IN LIFTS OF 8" OR LESS (LOOSE DEPTH).
4. MAXIMUM DRY DENSITY SHALL BE DETERMINED BY AASHTO T-99 OR OTHER APPROVED METHODS.
5. BACKFILL SHALL BE COMPACTED IN LAYERS UNTIL THE DENSITY IS NOT LESS THAN 95 % OF THE MAXIMUM DRY DENSITY.


SPAN	FILL HEIGHT	ACCEPTABLE MATERIAL INSIDE C.B.Z.	ACCEPTABLE MATERIAL OUTSIDE C.B.Z.
≤ 24'-0"	≥ 12'-0"	A1, A3	"
≤ 24'-0"	< 12'-0"	A1, A2, A3, A4	"
> 24'-0"	ALL	A1, A3	"

BACKFILL REQUIREMENTS


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(910) 787-4303
(910) 371-2671

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th, NC
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NY

<h1 style="margin: 0;">MORRIS FARM ACCESS BRIDGE OVER MIAMI & ERIE CANAL</h1>	
<h2 style="margin: 0;">WEST CHESTER OHIO</h2>	
Designed	C/S Project No.
Drawn	11218
Checked	
Date	
<h3 style="margin: 0;">C/S4</h3>	